

Farrington Golf Club, Marsh Lane, Ston
Easton, Wells, BS39 6TS

Construction Traffic Management Plan

12 June 2020

For and on behalf of
Towens Waste Management Ltd

Project Ref: 2020-540

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1 INTRODUCTION

1.1 LvW Highways Ltd has been appointed by Towens Waste Management Ltd to produce a Construction Traffic Management Plan to discharge a planning condition for the development of Farrington Golf Club, Marsh Lane, Ston Easton, Wells, BS39 6TS.

1.2 The proposed works at the Farrington Golf Club including new academy Course, new driving range, two new golf holes to North-West, Front 5 holes converted to 9 hole course, new Spa and Accommodation, new touring caravan park and amenities, conversion of existing driving range to accommodation and proposed car park extension.

1.3 Mendip District Council approved the planning application 2018/0577/FUL on 26 March 2018. Condition 7 reads as follows:-

Construction Traffic Management Plan (Pre-commencement)

No development shall commence unless a Construction Traffic Management Plan has been submitted to and approved in writing by the Local Planning Authority. The works shall be carried out strictly in accordance with the approved plan. The plan shall include:

- *Construction vehicle movements;*
- *Construction operation hours;*
- *Construction vehicular routes to and from site;*
- *Construction delivery hours;*
- *Expected number of construction vehicles per day;*
- *Car parking for contractors;*
- *A scheme to encourage the use of Public Transport amongst contractors; and*
- *Measures to avoid traffic congestion impacting upon the Strategic Road Network*

Reason: To ensure that safe operation of the highway and in the interests of protecting residential amenity in accordance with Policy DP7, DP8 and DP9 of the Mendip District Local Plan Part 1: Strategy & Policies 2006-2029 (Adopted 2014). This is a condition precedent because any initial construction or demolition works could have a detrimental impact upon highways safety and/or residential amenity.

1.4 The purpose of a Construction Traffic Management Plan (CTMP) is to ensure that the impacts of construction works on the public, in particular with respect to temporary interruptions to vehicular and pedestrian traffic, are considered by the developer and reviewed by the Council. The CTMP must ensure that public safety is maintained at all times and that where ever possible interruption to the use of public space is minimised.

1.5 The traffic management strategy for the project is one of minimising the interface where ever possible between public and site traffic, and including the staging of deliveries such that the volume of traffic is kept as even as possible and controlling the vehicular movements on the project.

1.6 Avoiding hazards and controlling the risks arising from the use of the vehicles in construction work is essential. The health and safety executive (HSE) expect to see traffic management plans that include:

- Planning and managing both vehicles and pedestrian routes;
- The elimination of reversing where possible;
- Safe driving and working practices;
- Protection of the public;
- Adequate vision and lines of sight;
- The provision of guidance signs and barriers; and
- Adequate parking and off-loading/storage areas to service the project.

Executive Summary

1.7 Towens Waste Management Ltd have developed this CTMP taking into account various items, including but not limited to, the concerns of the local community and county stakeholders, limiting traffic movements, Driver Code of Conduct and Considerate Contractors Scheme and impacts on the local area, environmental impact to the area and the restrictions involved in the practicalities of building the project.

- All Towens delivery vehicles will have the latest GPS satellite positioning hardware and software necessary so that tracking of vehicles can be undertaken. During the construction day, all vehicles will have radio or telephone contact with the site. This will be used as necessary, so that a coordinated arrival and departure pattern can be achieved and that all vehicles will be in contact with the site on their route;
- All delivery vehicles will be badged with the Towens motif – to enable rigorous monitoring and enforcement of driver behaviour.
- All speed limits through the residential area will be adhered to via the driver code of conduct. Failure to enforce this will become a disciplinary hearing.
- Advanced notification to the general public warning of the start of the project and new transport movements;
- Arrangements for regular road maintenance and cleaning, e.g. road sweeping in the vicinity of the site access point as necessary;
- Wheel cleaning / dirt control arrangements at key stages of construction;
- Provision of temporary signs and traffic control where necessary;
- It should be noted that Towens will operate strict policy to control driver behaviour both within the construction site and on the highway network. Towens will sign up to the Considerate Contractor Scheme – and will also agree, implement and enforce a Driver Code of Conduct (including strict controls on speed through Tatling End). Breaches of this Code of Conduct reported by the community will be disciplinary offences. Towens will ensure the Code of Conduct can be easily enforced by ensuring that all delivery vehicles accessing the site will be badged with the Towens motif.

1.8 The implementation of a Construction Traffic Management Plan will aim to encourage increased movements outside of times of peak pedestrian movements

and traffic when the road network is typically at its busiest if it is considered to be necessary.

2 CONSTRUCTION TRAFFIC ACCESS

2.1 The philosophy of minimising road use as far as reasonably practicable has continued through the construction planning and the development of the CTMP. All planning and procurement relating to large items of plant and their usage has been based around keeping these items on site for the duration of an activity, rather than the normal practice of plant coming and going from site to keep hire rates/standing time to a minimum. The main packages of work for the construction of the Buildings and Ancillary Construction Works are not let at this stage, so enquiries will go to local companies where possible to reduce the travelling of plant and staff throughout the county.

Small Building and Ancillary Construction Vehicle Access

2.2 All small materials and equipment will be delivered via Marsh Lane and offloaded within new secure compounds adjacent to the proposed works such as the new driving range building, the Spa and accommodation units and the touring caravan shower block. Within the secure site compound there will be an area allocated for site operative parking and visitor parking.

2.3 The material/equipment handling would then be moved using typical site machines from the safety of the secure compound, to the building site.

2.4 For the larger deliveries it is proposed that they will use an existing field access directly off the A37 (see below).

Large Vehicle Access

2.5 Due to the volume of material required for the land profiling to create the driving range, academy golf course and new golf holes etc. it is proposed to utilise an existing field access directly off the A37. The existing field access is some 190 to 200 metres south of the A37 junction with the A39. It lies just outside of the 50mph speed limit and is located almost directly opposite the Rush Hill Junction advanced direction sign.

2.6 The existing access will be improved to accommodate the turning movements of the heavy goods vehicles so they may enter and leave the site safely and in a forward gear. All the HGVs tipper lorries will utilise an existing route (as an temporary haul road) to access the golf course.

Construction of vehicle access

2.7 The existing vehicle access off the A37 will be constructed to county standards, with the first 10m measured back from the carriageway edge being constructed in a bound material. This is in order to prevent any loose material transfer to the highway which could result in a hazard to all users of the highway.

2.8 Drawings detailing the access construction specifications in line with the standard details are included in **Appendix A**.

3 ROUTE TO SITE

3.1 For the 'Building and Ancillary Construction' traffic access to the building sites will be via the Golf Club along Marsh Lane. All 'Land Formation' deliveries will be from

the A37. For vehicles exiting the site the route will be the reverse of the inbound movement.

- 3.2 All suppliers will be notified in advance of the agreed route and allowable delivery times in accordance with this CTMP.
- 3.3 All Towens construction tippers have the latest GPS navigation systems installed allowing a 'Controller' to monitor all vehicle positions and control vehicle movements. All the vehicles are also in 'hands free' radio and telephone contact with the 'Controller'. It is through using this system that vehicles will be staggered along the A37 so that the occurrence of two vehicles meeting is minimised.
- 3.4 A vehicle waiting area will be identified on site so that if a delivery vehicle is travelling to the site with its load then the empty vehicle can be controlled to wait on site until the other vehicle has arrived, if necessary.
- 3.5 No abnormal loads are expected on this contract. However, if required all abnormal (low loaders) movements will be planned in advance and will be done in accordance with an appropriate risk assessment. Where such vehicles are brought onto site, a Banksman will be utilised at all times.
- 3.6 A record of all materials and method of transportation will be kept and monitored and made available for the Client at all times. Records of deliveries have to be carefully controlled to comply with waste management and transfer legislation and this will be checked against this CTMP.
- 3.7 All of the above measures for bringing vehicles on to site will be adhered to when vehicles are leaving the premises.
- 3.8 Temporary signs will be located at 100m from the access junction with the A37 in both directions and remain in place for the duration of the construction works. A drawing detailing the temporary sign locations is included in **Appendix B**. The temporary signage will be erected by a Somerset Highway Authority approved contractor. No unauthorised person(s) are permitted to work within the highway.

Interface between on-site and off-site road movements.

- 3.9 Responsibility for construction traffic movement is that of the project manager who together with the security team will:
- Ensure that subcontractors and suppliers are adhering to procedures set out in the pre-site conditions by booking in deliveries giving the required notices.
 - Prevent unauthorised contractors parking and the congestion of traffic on local highways. All personnel in the team will be in contact with each other and site management who in turn will have mobile and telephone contact with the subcontractors.
 - Provide security at the site compounds.
 - Maintaining roads in a clean and safe condition.
- 3.10 It is recognised that not all deliveries can notify Towens Waste Management Ltd or Farrington Golf Club (as Client): such deliveries are generally using smaller vehicles (deliveries by third parties are on occasion simply not booked in). Deliveries from other contractors often have difficulty in providing accurate arrival times. The project manager will then manage the delivery situation with the priority to get the vehicle off the public road system and within the site. Other options open to the team are to send deliveries away (persistent offenders) or temporarily send

deliveries to a lorry park with a return time. Lorries will not be allowed to lay-up along or adjacent to public roads.

- 3.11 Wheel wash facilities will be provided when necessary to minimise the spread of material from the area of the excavation. In addition the site roads will be regularly cleaned. These steps will ensure that material will not be transferred to the public highway. Dust suppression measures will be implemented which will prevent the generation of fugitive dust.

4 ENVIRONMENTAL IMPACT ON THE AREA

- 4.1 The development proposals require importation of inert materials (including clay-bound materials for the engineering layer; restoration materials; and restoration soils).
- 4.2 All material will arrive on 8 wheel ridge bodied with a loaded axle weight of 32 tonne tipper trucks. A lorry of this size will typically carry 8-9m³ 'in the tight' and this equates to 19 tonnes per lorry.
- 4.3 The environmental impact to the area will be minimised by limiting traffic movements to an average of 15 deliveries a day during construction i.e. 30 vehicle movements per day. NB: all loads are to be covered. It is anticipated that some weeks will peak between 40 to 50 per day for 5 days but then nothing for the next 2 weeks.
- 4.4 Site construction works will be restricted to 07:30 to 18:00 hours on Mondays to Fridays and 07:30 to 13:00 hours on Saturdays. There will be no site construction or restoration activity at any time on a Sunday, Public or National Holiday.
- 4.5 Under no circumstances would lorries be allowed to lay up on the A37, A39, A362, Marsh Lane or the residential streets of Farrington Gurney accept in authorised laybys. To prevent this from happening:
- Delivery hours will be limited to working hours. This condition forms part of sub contract documentation, and subcontractors are reminded of this at pre-start meetings.
 - Deliveries are booked in on site, and logged on the central register.
 - Security/gates men will be in position half an hour before start of work and before the earliest delivery time.
 - Persistent offenders will be reported to the project managers, who will action with the directors of the offending company.
- 4.6 To limit the number of vehicles on the site at any one time the following will be undertaken:
- Provide car and van parking for the workforce and visitors away from the work area;
 - Controlled entry to the work area; and
 - Planned storage areas so that delivery vehicles do not have to cross the site.
- 4.7 Fuel consumption will be minimised by using local materials and sub-contractors. Towns Waste Management Ltd has strategies in place to reduce both noise and dust emissions during the construction processes.
- 4.8 Introduction and enforcement of Good Driving Code of Practice for all construction traffic to reduce vehicle noise exposure to residents. Construction vehicle fleet to

be 'in-house' Towens vehicles or badged with a Towens motif. The Good Driving Code of Practice will allow effective policing to ensure good/considerate driving styles (e.g. through limiting speed, enforcing good driving behaviour, regular maintenance of fleet).

4.9 'Land Formation' construction traffic will be prohibited from using Marsh Lane, thus resulting in limited traffic impact from the 'Building and Ancillary Construction' traffic in this locality.

4.10 The large area of the site will ensure that construction works will only take place for relatively short periods at the closest part of the site to the nearest residences in question. Phasing of construction works will ensure that works do not take place close to the same residences at all times of the construction programme. The result will be that the worst-case assessment considered will only take place for a relatively short period of the entire construction programme. The substantial distances involved between construction area and residence concerned will enable construction noise to readily attenuate over distance.

Design

4.11 Throughout the design and development of the Farrington Golf Club scheme, traffic management has been an integral part of the proposed solution. Measures include:

- Inclusion of a delivery vehicle waiting point on site within the compound area so that staggered vehicle arrival and departures can be organised;
- There will be no need to remove material from site, only importing material for re-grading the land.
- The site will operate a phasing design and location of construction compound so that the impact of vehicle movements within the site has been minimised.

5 TRANSPORT OF PERSONNEL TO SITE

5.1 In order to minimise the impact of personal car use, Towens Waste Management Ltd will recommend that construction personnel will car share as far as is practicable. This will minimise traffic movement in the area.

5.2 Each site compound will accommodate up to 2 no. temporary port-a-cabin-type buildings in addition to an area for material storage. These port-a-cabins are required for offices, toilets, canteen and storage and each will be between 6 metres to 8.5 metres in length.

5.3 Facilities to eat food and drink on site will be provided to ensure personnel do not need to leave the site during the working day, which again minimises the traffic through the local community.

5.4 Towens Waste Management Ltd will ensure that where possible local labour and sub-contractors will be utilised for the duration of this project, thus minimising general traffic through the region.

5.5 Where off site road usage is essential, all employees, sub-contractors and supply-chain members will be advised in the induction and subcontracts of the need to adhere to local speed limits, use the dedicated route to and from site and will be made aware of potential risks associated with the local 'school-run' and other commercial activities in the area.

- 5.6 Assessments will be carried out to determine the need for travel to the construction site and where possible (for example through the use of tele/video conferencing) such travel will be minimised.
- 5.7 Towns Waste Management Ltd will encourage personnel to use public transport or cycle to travel to site. Secure facilities will be provided for personnel arriving by bicycle.

6 TRANSPORT OF MATERIALS, PLANT AND EQUIPMENT TO SITE – CONSTRUCTION PROGRAMME

- 6.1 Road deliveries will be timed to avoid the busiest rush hour periods whenever practicable.
- 6.2 On commencement of development, a number of road deliveries will be required to prepare the site. Loads required for site establishment are detailed in **Appendix C**. Mitigation measures will be taken to ensure that any impacts from these deliveries are minimised. Appendix C also includes a sample list of the major plant that will be required for the ‘Land Formation’:-

Staff numbers

Building construction

- 6.3 It is anticipated that for much of the build it is estimate that there will be under 10 staff on site. For the new building construction which is the later phase of the proposal, there will be approximately 20 to 30 construction staff on site with the exact number depending on each phase of construction.
- 6.4 It is considered that there will be a degree of vehicle sharing by construction staff, or staff may be offered the potential to travel to the site in minibus from local population centres such as Midsomer Norton, Wells and Shepton Mallet, such that the number of staff trips is reduced.
- 6.5 A temporary parking area will be provided within the secure site compound for construction staff, adjacent to the materials delivery compound.

Land formation

- 6.6 It is anticipated that there will be approximately 5 to 10 ground works staff and machine operators on site at any one time, with the exact number depending on the volume of fill material made available.
- 6.7 It is anticipated that all construction operations will be undertaken between 0730 and 1800 Monday to Friday and 0730 to 1300 on Saturday.

Mitigation measures

- 6.8 Construction and delivery vehicles accessing the site will be required to follow a construction routeing strategy using the route identified in order to minimise impacts on the surrounding highway network.
- 6.9 The route will be from the A37 for the ‘Land Formation’ traffic and along Marsh Lane for the ‘Building and Ancillary Construction’ traffic. All suppliers and contractors will be notified of the agreed construction routeing strategy and expected to comply.
- 6.10 A banksmen will be stationed at the site access to ensure that deliveries do not attempt to depart the site when another vehicle is due to arrive. The banksmen will also ensure there is no conflict with vehicles on the access roads.

- 6.11 No vehicle parking, loading or unloading will take place from the public highway and suitable wheel washing facilities will be installed, if required, to ensure no mud or debris is deposited on the public highway during the construction period.
- 6.12 Temporary advance signage will be placed on the A37, 100m to either side of the access junction with the A37 warning approaching motorists of the presence of a construction site accesses and slow moving vehicles.

Road Condition Survey

- 6.13 If required by the Local Authority, Towens Waste Management Ltd or Farrington Golf Club can undertake a Road Conditions Survey on the proposed construction route along the A37 and Marsh Lane. It would be proposed to undertake a conditions survey before, during and after construction, to ensure that any damage caused by construction vehicles associated with the development was recorded, thus allowing the Golf Park operator to fulfil their responsibility by rectifying any damage.
- 6.14 The survey methodology would be a photographic image report and would be prepared as a source of evidence should any damage claims arise.

Environmental Protection

- 6.15 Potential impacts of construction traffic including noise, vehicle exhaust emissions, dust, and mud and debris on roads will be mitigated through strict adherence to the proposed construction routes and permitted hours of working.
- 6.16 Controls under health and safety legislation and good construction site practices, will also be adhered to, such as wheel wash facilities on exiting the site and, if required, regular cleaning of the A37 and Marsh Lane to ensure there are no mud deposits left on the delivery route.

Monitoring

- 6.17 The measures detailed within this report will be monitored by the contractor to ensure they are adhered to and any potential negative effects arising from the construction deliveries are prevented.
- 6.18 It is in the interest of the construction programme that deliveries are not affected by existing traffic movements and therefore construction routes, delivery timings and access arrangement will be continually recorded and reviewed throughout the

7 RISK / HAZARD / ACTION MATRIX

RISK	PEOPLE AFFECTED	EXISTING CONTROL MEASURES	FURTHER PREVENTATIVE ACTION REQUIRED
Collision with vehicles not related to construction	All those with vehicles within our sphere of operations	Existing speed limits Warning signs	Issue instructions to all delivery companies and staff that crawling speeds are mandatory with hazard lights turned on. Use of banksmen to guide delivery vehicles
Collision with vulnerable users beside the site	Any visitor to the fields beside the trackway and crew operatives driving or walking to site	Relatively few people are likely to be around when any traffic movements are taking place. There are no footways on the site entrance side of the road so this will minimise the risk of meeting pedestrians walking beside where entry to the site will be from.	Placement of construction signs warning of works close to the site entrance. Approaching the construction site entrance, drivers to switch on hazard lights. When on the haul road drivers will reduce speed to a crawl and only move with the aid of a banksman
Transfer Vehicles getting stuck on site	All transfer vehicles and site crew vehicles	The approach to the site is a field track, this will be laid immediately this is left, drivers will be on a temporary steel trackway	Use a banksman in and out of site and ensure drivers stay on the haul road. Request delivery drivers to turn around when in compound and drive back out of the compound on the haul road facing forward
Delivery vehicles cannot gain entry due to parked cars	All delivery vehicles and site crews	While there are NO no parking signs beside the compound, or road markings prohibiting	No further action required other than placing a sign on the compound fencing at the entrance to the

Delivery vehicles damaging buried services	The owners of the buried services	parking, cars do NOT park on the road. The site has had services checks carried out and all known services have been identified and the invert levels are known. The site has also been CAT scanned to verify the positioning of services, and they match the records received with no others on site identified that were otherwise unknown	haul road stating not to park in front of the gates No services affected. If found, to further protect the services Aluminium track way will be laid over these service runs.
Keeping the highway free of site generated mud and debris	All road users	The junctions of the field access where it intersects with the A443 will be constructed of Type 1 GSB on a membrane.	A water bowser and pressure washer will be on site to clean any delivery vehicle wheels if it is needed to be done to protect the highway.
Control of traffic while delivery vehicles are Entering and exiting the site	All traffic using the A37 and Marsh lane at the time of egress from site	A qualified Banksman wearing PPE will be controlling entry and egress of delivery vehicles. Copies of training certificates will be held in the site compound for inspection.	Banksmen will utilise Chapter 8 STOP GO boards to control motorists on the A37 while the vehicles are exiting the site only if required.
Contractor vehicle parking and material storage – ensuring there is no disruption to local residents	All members of construction company and local residents	Crews will share vehicles to get to site. Crews are not permitted to park on the road.	No further control measures needed. There is adequate car parking within the compound for all crew and for material storage, materials will be unloaded within the compound also.
24 hour Emergency Contact details	Anybody with the need to make	Adequate signage giving emergency contact numbers will	The design and content of this board will be agreed with

<p>Adequate insurance for works on site is needed, specifically Liability Insurance for £10million</p> <p>Damage to the highway or kerb stones at entry / exit point</p>	<p>emergency contact with the contractor</p>	<p>be displayed in various positions around the site attached to the fence line.</p>	<p>the client but an example board has been provided within this document.</p> <p>The numbers will be for the Construction company director/s and the site manager (TBC).</p>
	<p>Users of the highway</p>	<p>A copy of the insurance certificate will be held in the site compound for inspection.</p>	<p>A photographic condition survey will be taken by Towens Waste Management Ltd or Farrington Golf Club of the approach to and the entry point to the site and copies supplied to The Highways Department before works commence if required.</p>

Clarification of proposed delivery times

Typically speaking, building material deliveries are requested at 07:30 am to avoid rush hour periods and if a second delivery in a day is needed, it is usually around 11:00, but two deliveries a day are rare. It is exceptionally rare to have any deliveries or collections past 15:00 and they should not be requested to be after this hour.

Welfare Units, Storage Container and Plant deliveries can be requested to miss the usual school rush hours.

Additional material requirements will also be requested to do the same. They are requested to fit in with site requirements.

8 EXAMPLE OF SITE WARNING SIGNS

EXAMPLE WARNING SIGNS TO ALERT VISITORS AND OTHER ROAD USERS TO THE POTENTIAL OF CONSTRUCTION TRAFFIC MOVEMENTS



NOTE: As shown, the signs will display 24hr emergency contact details for both Towns Waste Management Ltd director and the site manager.

9 TYPICAL DELIVERY/CONSTRUCTION VEHICLE DETAILS

VEHICLE TYPE

PHOTO (A TYPICAL EXAMPLE)

Skip Wagon



6 - 8m³ Concrete Wagon



16 - 32T Tipper Lorry (for Stone and Soil)



Roll on and Roll Off Trailer and Cab.
Deliveries of Excavator, Dumper, Roller and
Concrete Pump



Steel Container Delivery lorry (with
telehandler / Hoist)



12ft Towable Welfare Typically towed to site
by a small 4wd Truck such as a Land Rover
or similar



Fencing Delivery van A typical example



Block and sundries supplies delivery vehicle
3 axle Flatbed truck with tele handler hoist.



Site Crew LWB Transit van



80 – 120 Ride on Roller



3 - 8T Tracked Excavator



3 - 6T Swivel Dumper



Trailer Mounted concrete pump Either delivered with other plant on the RoRo flatbed or towed using a 4wd truck such as a Land Rover



Trailer Mounted Air compressor Either delivered with other plant on the RoRo flatbed or towed using a 4wd truck such as a Land Rover

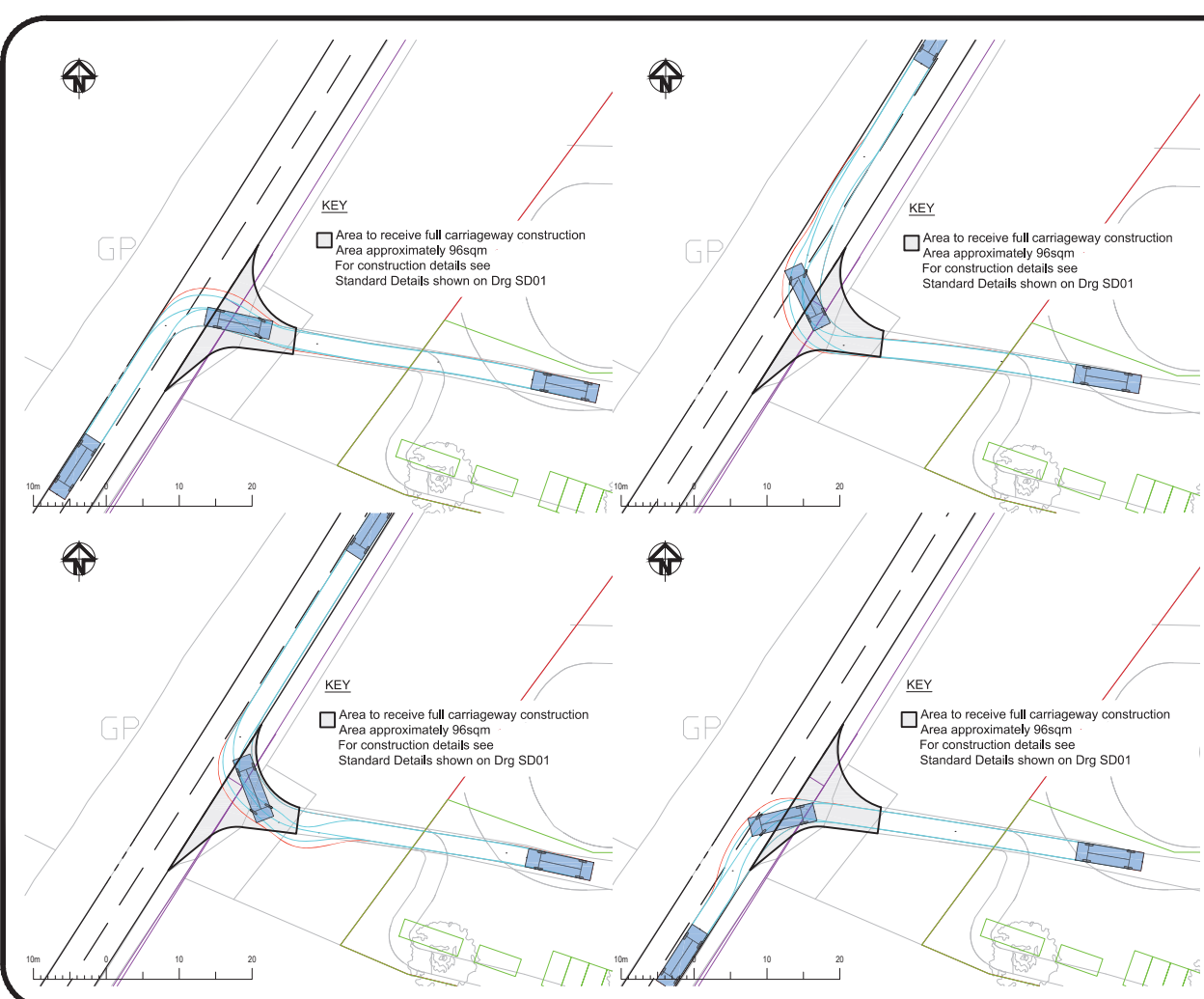


Trailer Mounted water bowser and pressure washer Either delivered with other plant on the RoRo flatbed or towed using a 4wd truck such as a Land Rover



APPENDIX A

A37 Access drawings



General Notes

1. ALL DIMENSIONS ARE INDICATIVE AND IN METRES UNLESS NOTED OTHERWISE.
2. DO NOT SCALE THIS DRAWING. USE FIGURED DIMENSIONS ONLY.
3. ALL LEVELS ARE IN METRES ABOVE ORDNANCE DATUM (AOD) BASED ON THE TOPOGRAPHICAL SURVEY DRAWING SUPPLIED BY THE CLIENT.
4. ANY DISCREPANCIES TO BE REPORTED TO THE ENGINEER.
5. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT ARCHITECTS AND ENGINEERS DRAWINGS AND SPECIFICATIONS.
6. THE SPECIFICATION IN ALL ASPECTS SHALL BE IN ACCORDANCE WITH THE CURRENT RESIDENTIAL DESIGN GUIDE AND SPECIFICATION IN FORCE WITHIN THE COUNTY AT THE TIME OF CONSTRUCTION.
7. COPYRIGHT OF THIS DRAWING IS RETAINED BY THE ENGINEER AND IT MUST NOT BE REPRODUCED IN WHOLE OR PART WITHOUT WRITTEN CONSENT.

Maximum Speed: 40km/h (25mph)
 Towns: S11 Super Lorry (Based on Volo-P90)
 20' Road High (5' 6" High - 8' ride (2.5m))
 Overall length: 21.00m
 Overall width: 2.55m
 Max. Axle Spacing: 9.14m
 Max. Axle Weight: 40.00t
 Max. Axle Spacing: 9.14m
 Max. Axle Weight: 40.00t
 Max. Axle Spacing: 9.14m
 Max. Axle Weight: 40.00t
 Max. Axle Spacing: 9.14m
 Max. Axle Weight: 40.00t

A For Discussion	14.10.2003
No.	Revision/Issue
	Date

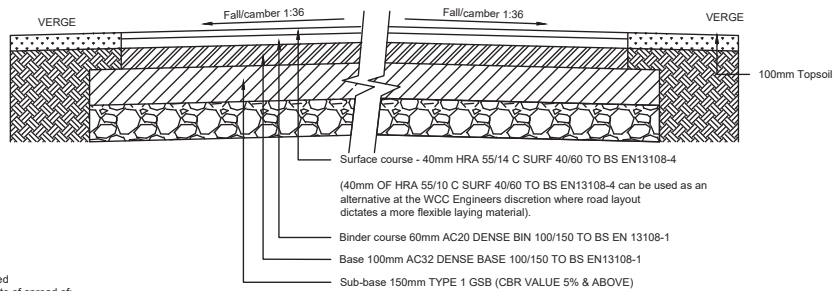
Project Name and Address

LvW Highways Ltd
 Engineers
 Farnham, Surrey
 GU14 7PH
 Tel: 01257 990799
 Fax: 01257 224695

Project Name and Address

Farrington Golf Club
 Marsh Lane, Ston Easton
 WELLS, BS39 6TS
 Site Compound and
 Vehicle Swept Path

Project: 2020-540	Sheet:
Date: 10.06.2020	AP01
Scale: 1:500@A3L	

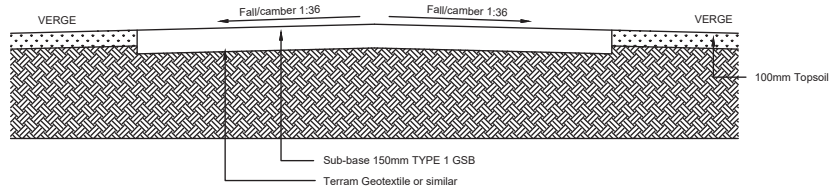


A bond coat in accordance with BS EN 13808 should be applied between all bound layers. Bond coat should be applied at a rate of spread of-

- at least 0.35 Kg/m² of residual binder for planned or milled surfaces
- at least 0.2 Kg/m² residual binder to newly laid or overlaying existing asphalt

CBR VALUE	CAPPING LAYER THICKNESS	TYPE 1 GSB
5% AND ABOVE	N/A	225mm
2% > 5%	350mm	150mm
- 2%	600mm	150mm

ACCESS BELLMOUTH CONSTRUCTION



ACCESS ROAD CONSTRUCTION

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A For Discussion		
No.	Revision/Issue	Date

Prepared Name and Address

LvW Highways Ltd
 Farrington Golf Club
 Marsh Lane, Ston Easton
 WELLS, BS39 6TS
 Site Access Standard Details
 Tel: 01781 307000
 Fax: 01781 228805

Project Name and Address

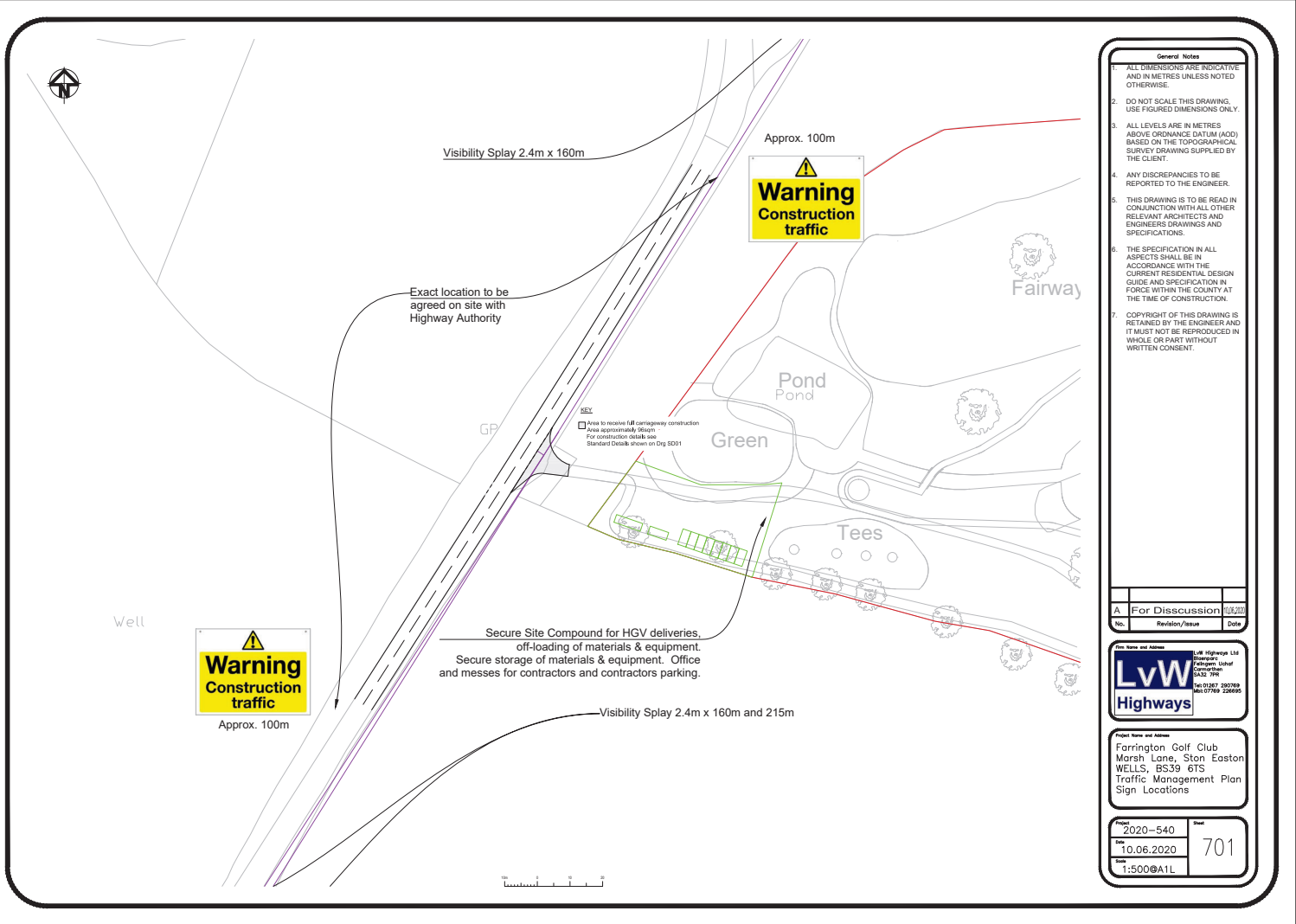
Farrington Golf Club
 Marsh Lane, Ston Easton
 WELLS, BS39 6TS
 Site Access Standard Details

Sheet	2020-540	Date	
Date	10.06.2020	Scale	SD01
Drawn	N.T.S. @A4L		

540c-dwg 14.10.2003 Karl_von_Weber

APPENDIX B

Plan showing suggested temporary sign locations



- General Notes**
1. ALL DIMENSIONS ARE INDICATIVE AND IN METRES UNLESS NOTED OTHERWISE.
 2. DO NOT SCALE THIS DRAWING. USE FIGURED DIMENSIONS ONLY.
 3. ALL LEVELS ARE IN METRES ABOVE ORDNANCE DATUM (AOD) BASED ON THE TOPOGRAPHICAL SURVEY DRAWING SUPPLIED BY THE CLIENT.
 4. ANY DISCREPANCIES TO BE REPORTED TO THE ENGINEER.
 5. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT ARCHITECTS AND ENGINEERS DRAWINGS AND SPECIFICATIONS.
 6. THE SPECIFICATION IN ALL ASPECTS SHALL BE IN ACCORDANCE WITH THE CURRENT RESIDENTIAL DESIGN GUIDE AND SPECIFICATION IN FORCE WITHIN THE COUNTY AT THE TIME OF CONSTRUCTION.
 7. COPYRIGHT OF THIS DRAWING IS RETAINED BY THE ENGINEER AND IT MUST NOT BE REPRODUCED IN WHOLE OR PART WITHOUT WRITTEN CONSENT.

A	For Discussion
No.	Revision/Phase
Date	Date

Firm Name and Address: LW Highways Ltd
 25 High Street, London, SE17 1NF
 Telephone: 020 7277 5000
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Firm Name and Address:
 Farrington Golf Club
 Marsh Lane, Ston Easton
 WELLS, BS39 6TS
Traffic Management Plan Sign Locations

Project: 2020-540 Date: 10.06.2020 Scale: 1:500@A1L	Sheet <div style="font-size: 2em; font-weight: bold;">701</div>
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APPENDIX C

Schedule of Planned Principal Road Deliveries

Farrington Golf Club, Marsh Lane, Ston Easton, Wells, BS39 6TS
Schedule of Planned Principal Road Deliveries

MATERIALS	USE ON PROJECT	EXPECTED FREQUENCY
Contractors accommodation 2 Cabins – 1 vehicle 1 generator – 1 vehicle 2 storage containers – 1 vehicle	Welfare facilities, Power to welfare units, Storage of COSHH materials, safety equipment, small tools and equipment, etc.	2 weeks at start and end of project
Land Formation 32tonne tipper, Inert soil fill material	Land formation for golf course features etc.	Average 15 loads per day. Some weeks may peak at 40 to 50 per day but then nothing for the next 2 weeks.
Ready Mixed Concrete 50 loads	General construction use mainly foundations	Within 12 months period
Fuel Deliveries Diesel – 1800 loads	Fuelling plant	Average 1 per week for duration of project
General Building Materials 175 loads	Buildings and ancillary construction works	Throughout contract average 2 per day
Large Plant and Equipment Dozers – 1 load Excavators – 3 loads Temporary pumps – 1 loads Roadwork equipment, fences and barriers – 3 loads Rollers – 2 loads Dumper – 2 loads Mobile cranes – 20 loads Bulldozer - Caterpillar D6 or equivalent Bulldozer - Caterpillar D8 or equivalent 20 tonne excavators	General site work General site work Dewatering if required On site traffic management system Earthwork compaction General building works Various lifting activities Land Profiling Land Profiling Land Profiling	Within first month/last month Throughout contract Throughout contract Within first month/last month Throughout contract Throughout contract Throughout contract Within first month/last month Within first month/last month Within first month/last month

Farrington Golf Club, Marsh Lane, Ston Easton, Wells, BS39 6TS
 Schedule of Planned Principal Road Deliveries

25 tonne dump trucks Screener Sheep's foot roller Other machinery may be required for short periods	Land Profiling Land Profiling Land Profiling	Within first month/last month Within first month/last month Within first month/last month Throughout contract
Small routine deliveries- small vans Waste disposal Catering supplies General construction ancillaries PPE, signs, etc.		Estimated 20 vans per day in/out